THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Contact Info: Rachel H. Cole, P.E.
<rachel.cole@arcadis.com>
Date: 2025.03.27 11:15:17-04'00'

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ARCADIS US, INC.
524 SOUTH GAY STREET, SUITE 201
KNOXVILLE, TENNESSEE 37902
RACHEL H. COLE, P.E. NO: 119667

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY AND TRAFFIC DESIGN DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
TYPICAL SECTIONS	2B1
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E
TABULATED QUANTITIES	2F
UTILITY NOTES AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1

YEAR	PROJECT NO.	SHEET NO.
2025	NH/HSIP-111(129)	ROADWAY-SIGN1
	88S111-F8-002	
	88S111-F3-002	

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH/HSIP-111(129)	
STATE PROJ. NO.	88S111-F8-002, 88S111-F3-002	
BRIDGE	88S111-M3-002	

VAN BUREN COUNTY

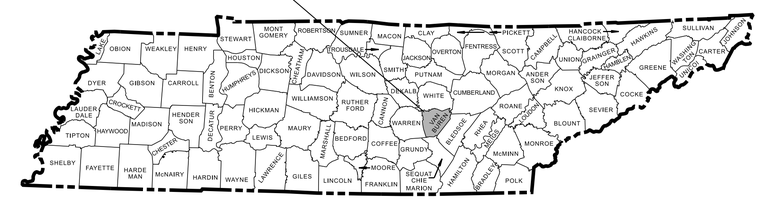
STATE ROUTE 111:
FROM NEAR SPARTA STREET (L.M. 14.87)
TO WHITE COUNTY LINE (L.M. 19.54)

RESURFACE & SAFETY

BRIDGE REPAIR, MICRO-SURFACING @ 22 LBS/SY, HP FOG SEAL SHOULDERS, AND PAVEMENT MARKINGS

STATE HIGHWAY NO. 111 F.A.H.S. NO. N/A

PROJECT LOCATION
BRIDGE ID. # 88SR1110007 , 88SR1110008

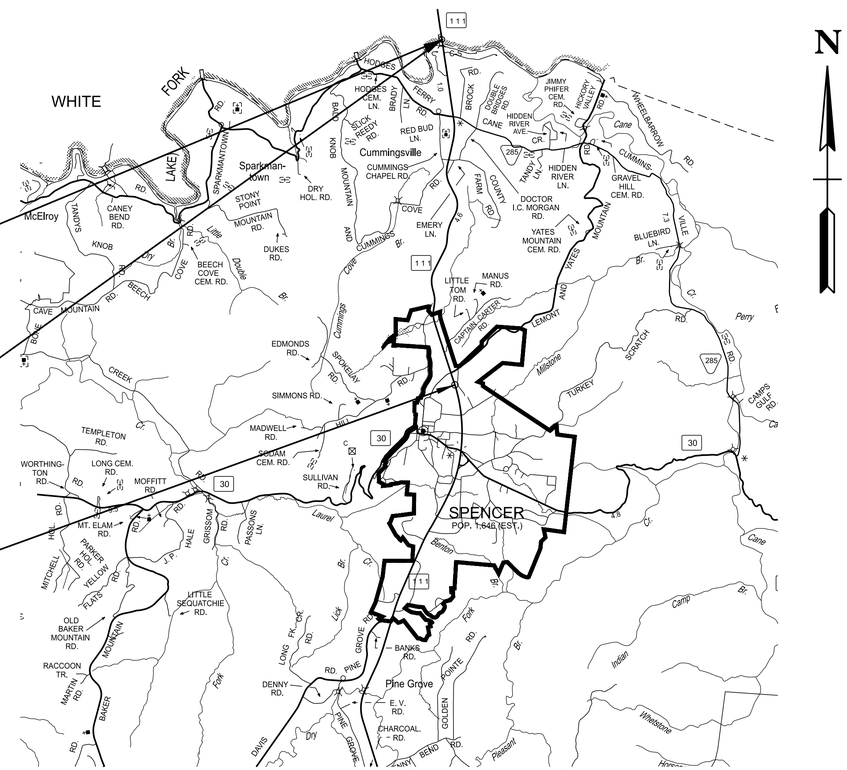


NO EXCLUSIONS

88S111-F8-002
88S111-F3-002
END PROJECT NO. NH/HSIP-111(129) RESURFACE & SAFETY
L.M. 19.54

CANEY FORK RIVER BRIDGE REPAIR PROJECT NO. 88S111-M3-002
L.M. 19.51

88S111-F8-002
88S111-F3-002
BEGIN PROJECT NO. NH/HSIP-111(129) RESURFACE & SAFETY
L.M. 14.87



SCALE: 1"= 5280'

PROJECT LENGTH	4.67 MILES
TOTAL LANE MILES RESURFACED	19.74 MILES

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : ANDREW ZAZZARA, P.E.
DESIGN FIRM : ARCADIS US, INC.
DESIGNER : ASHKAN NESHAGARAN, E.I. CHECKED BY RACHEL H. COLE, P.E.
P.E. NO. 98023-4217-04 (DESIGN)
PIN NO. 134123.00

TRAFFIC DATA	
ADT (2025)	6378
POSTED SPEED	
LM 14.87 - LM 15.30	45 MPH
LM 15.30 - LM 19.54	55 MPH

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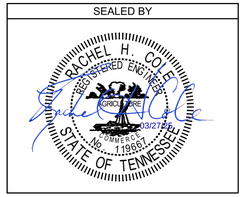

APPROVED:
WILL REID, DEPUTY COMMISSIONER / CHIEF ENGINEER

DATE:

APPROVED:
HOWARD H. ELEY, DEPUTY GOVERNOR & COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED:

DIVISION ADMINISTRATOR DATE

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	1A
		88S111-F8-002	
		88S111-F3-002	

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY AND TRAFFIC DESIGN DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
TYPICAL SECTIONS	2B1
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E
TABULATED QUANTITIES	2F
UTILITY NOTES AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1
BRIDGE PLANS	B1

NOTES:

THE ALPHABETICAL LETTERS “I”, “O”, “Q” ARE NOT USED IN NUMBERING OF SHEETS.

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
10-100.00 STANDARD ROADWAY TITLE SHEET. ABBREVIATIONS, AND LEGENDS		
RD-TP-1	10-01-24	STANDARD ROADWAY DRAWINGS TITLE SHEET
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
10-204.00 DESIGN - TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS, AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

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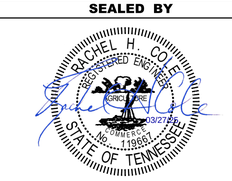

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX,
STANDARD ROADWAY
AND
TRAFFIC DESIGN
DRAWINGS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	1B
		88S111-F8-002	
		88S111-F3-002	

PROJECT COMMITMENTS

COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	Environmental Division, Hazardous Materials	Asbestos Containing Material (ACM) surveys were completed on Bridge No. 88SR1110007 SR-111 NB over Caney Fork River LM 19.51 (88-SR111-19.51R) and Bridge No. 88SR1110008 SR-111 SB over Caney Fork River LM 19.51 (88-SR111-19.51L). No asbestos was detected. Please see the reports for further details and photographs. No special accommodations for demolition and waste disposal are anticipated for these structures and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	bridge

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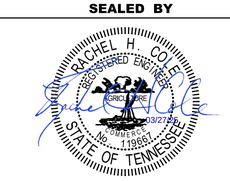

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	2
		88S111-F8-002	
		88S111-F3-002	

ESTIMATED ROADWAY QUANTITIES

	ITEM NO.	DESCRIPTION	UNIT	QUANTITY 88S111-F8-002	QUANTITY 88S111-F3-002	TOTAL QUANTITY
(1)	208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	16		16
(2)	403-01.01	BITUMINOUS MATERIAL FOR TACK COAT (MICRO-SURFACING)	TON	19		19
(3)	403-01.10	HIGH PERFORMANCE FOG SEAL	S.Y.	90490		90490
(4)	414-03.01	EMULSIFIED ASPHALT FOR MICRO-SURFACING	TON	187		187
(5)	414-03.02	AGGREGATE FOR MICRO SURFACING	TON	1556		1556
(6)(7)	705-04.20	GUARDRAIL DELINEATION ENHANCEMENT	EACH		3850	3850
	712-01	TRAFFIC CONTROL	LS	1		1
	712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	150		150
	712-05.03	WARNING LIGHTS (TYPE C)	EACH	100		100
(7)	712-06	SIGNS (CONSTRUCTION)	S.F.	1064		1064
	712-08.03	ARROW BOARD (TYPE C)	EACH	2		2
	713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	2		2
	716-01.21	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	168		168
	716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	684		684
(8)	716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	852		852
	716-01.40	REMOVE AND REPLACE LENS ON SNOWPLOWABLE REFLECTIVE MARKER	EACH	143		
(9)	716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.		114	114
(9)	716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.		406	406
(9)	716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH		22	22
(9)	716-02.08	PLASTIC PAVEMENT MARKING (8" DOTTED LINE)	L.F.		862	862
(10)	716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	22		22
(10)	716-05.21	PAINTED PAVEMENT MARKING(4"DOTTED LINE)	L.F.	3168		3168
	716-08.04	REMOVAL OF PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	114		114
(11)	716-08.05	REMOVAL OF PAVEMENT MARKING (STOP LINE)	L.F.	164		164
	716-08.06	REMOVAL OF PAVEMENT MARKING (TURN LANE ARROW)	EACH	22		22
(12)	716-08.20	REMOVAL OF PAVEMENT MARKING (LINE)	L.M.	5.2		5.2
(13)(14)	716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	22		22
(14)	716-12.06	ENHANCED FLAT LINE THERMO (8IN LINE)	L.F.	786		786
	717-01	MOBILIZATION	LS	1		1

FOOTNOTES

(1) INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET NO. 2C, FINAL PAVEMENT MARKING NOTE (6) FOR MORE INFORMATION. INCLUDES CLEANING EXISTING CATCH BASINS AS DIRECTED BY TDOT ENGINEER.

(2) NO PAYMENT WILL BE MADE DIRECTLY FOR THE WATER REQUIRED TO DILUTE THE TACK COAT.

(3) CONTRACTOR TO EXERCISE CAUTION WHEN APPLYING THE FOG SEAL ADJACENT TO THE BARRIER WALL FROM L.M. 15.93 TO L.M. 17.54 IN BOTH THE NORTHBOUND AND SOUTHBOUND DIRECTIONS.

(4) INCLUDES 4 TONS FOR TURN LANES AND CROSSOVERS.

(5) INCLUDES 31 TONS FOR TURN LANES AND CROSSOVERS.

(6) DELINEATORS SHALL BE:
- "HINGED-TYPE" DELINEATORS FROM TDOT'S CURRENT QPL.
- MOUNTED TO ALL GUARDRAIL POSTS EXCEPT END TERMINAL SECTIONS.
- ATTACHED TO GUARDRAIL POST AS PER MANUFACTURER'S INSTRUCTION.
- ATTACHED USING MANUFACTURER'S RECOMMENDED ADHESIVE.

(7) SEE SHEET 2F FOR TABULATED QUANTITIES.

(8) TO BECOME PROPERTY OF THE CONTRACTOR. INCLUDES ALL COST ASSOCIATED WITH PROPER DISPOSAL OF SNOWPLOWABLE MARKERS.

(9) CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.

(10) RESURFACING QUANTITY TO BE USED FOR TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS.

(11) ITEM FOR REMOVAL OF STOP LINES IN CROSS-OVERS.

(12) FOR REMOVAL OF LANE LINE MARKINGS ONLY.

(13) INCLUDES QUANTITY FOR RESTRIPING EXISTING EDGELINES AFTER HP FOG SEALING OF SHOULDERS ARE COMPLETE.

(14) CONTRACTOR SHALL USE THE EXTRUDED OR RIBBON METHOD FOR APPLICATION.

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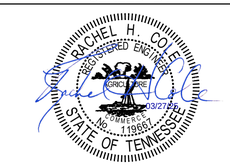

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED ROADWAY QUANTITIES

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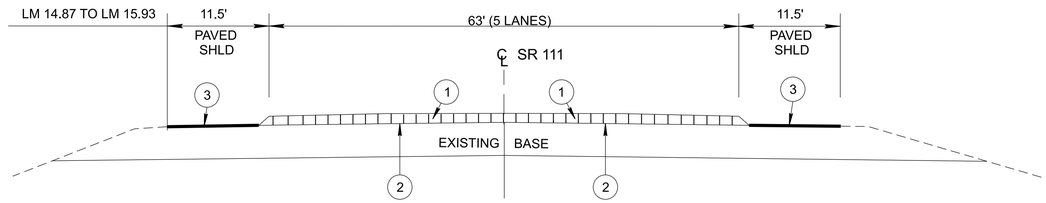
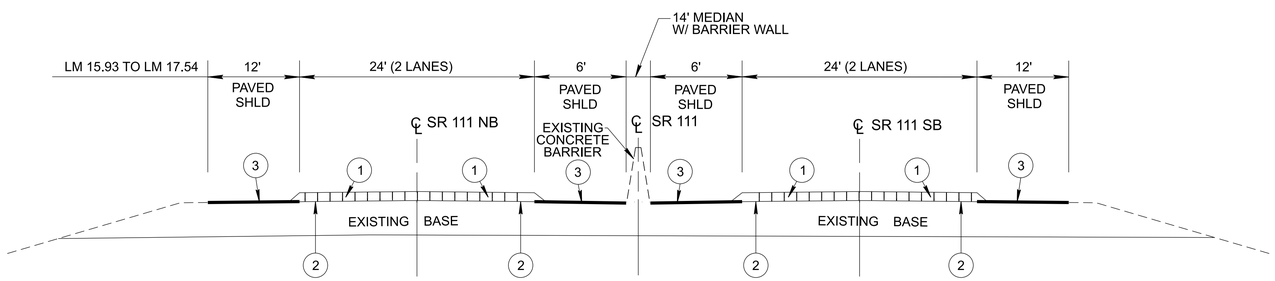
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	2B
		88S111-F8-002	
		88S111-F3-002	



SR-111
TYPICAL SECTION

(MICRO-SURFACE FROM INSIDE EDGELINE TO INSIDE EDGELINE)



SR-111
TYPICAL SECTION

(MICRO-SURFACE FROM INSIDE EDGELINE TO INSIDE EDGELINE)

THE CONTRACTOR SHALL USE EXTREME CAUTION WHEN SPRAYING FOG SEAL NEXT TO THE BARRIER WALL TO PREVENT SPRAYING ON BARRIER WALL.

PROPOSED PAVEMENT SCHEDULE

(1) MICRO-SURFACING @ 0.275 " THICK (APPROX. 22 LB./S.Y.)

414-03.01 EMULSIFIED ASPHALT FOR MICRO-SURFACING @ 12% OF THE DRY WEIGHT OF AGGREGATE

414-03.02 AGGREGATE FOR MICRO-SURFACING @ 22 LBS/S.Y.

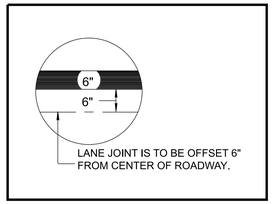
(2) TACK COAT (MICRO-SURFACING) @ 0.10 TO 0.15 GAL./S.Y.

403-01.01 BITUMINOUS MATERIAL FOR TACK COAT (TC) FOR MICRO-SURFACING, TON

(3) HIGH PERFORMANCE FOG SEAL (SHOULDERS)

403-01.10 HIGH PERFORMANCE FOG SEALS @ 0.10 TO 0.15 GAL./S.Y. , S.Y.

NOTE: PERMANENT PAVEMENT EDGELINE MARKING SHALL BE APPLIED AFTER HP FOG SEALING SHOULDERS IS COMPLETE.



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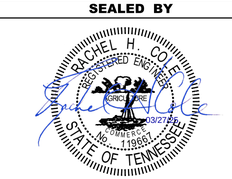

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS AND PAVEMENT SCHEDULE

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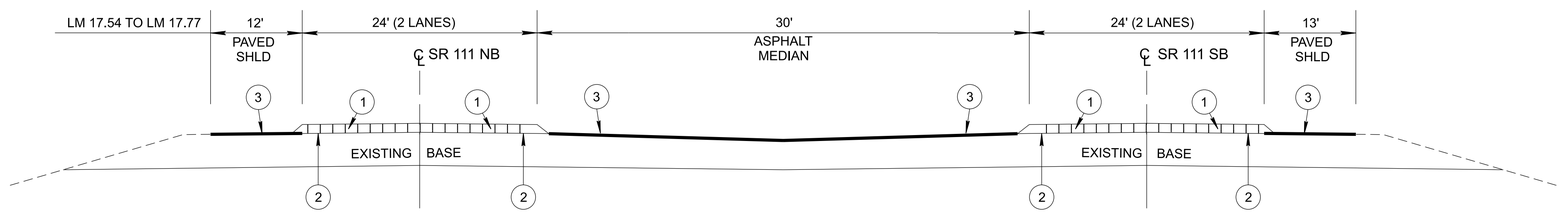
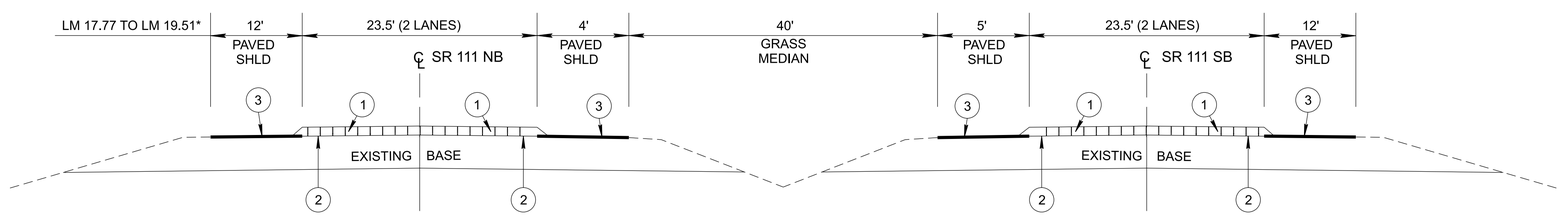
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	2B1
		88S111-F8-002	
		88S111-F3-002	



SR-111
TYPICAL SECTION
(MICRO-SURFACE FROM INSIDE EDGELINE TO INSIDE EDGELINE)



* FROM L.M. 19.51 TO LM. 19.54
EXCLUDE MICRO-SURFACE / FOG SEAL ON BRIDGE OVER CANEY FORK RIVER.
517'-6" LEFT AND 486'-10" RIGHT.
(SEE BRIDGE SHEETS FOR DETAILS OF BRIDGE DECK TREATMENT.)

SR-111
TYPICAL SECTION
(MICRO-SURFACE FROM INSIDE EDGELINE TO INSIDE EDGELINE)

ADDITIONAL MICRO-SURFACE CROSS-OVERS WITH TURN LANES			
DIRECTION	LM TO LM	WIDTH	AREA (SY.)
SB	19.463 - 19.423	12'	281.60
SB	18.620 - 18.578	12'	295.68
SB	18.056 - 18.000	12'	394.24
NB	17.932 - 17.978	12'	323.84
NB	18.507 - 18.559	12'	366.08
NB	19.348 - 19.402	12'	380.16
CROSS-OVER	@ LM 19.410		347.28
CROSS-OVER	@ LM 18.564		347.28
CROSS-OVER	@ LM 17.985		347.28
TOTAL			2,794.33

ADDITIONAL FOG SEAL CROSS-OVERS W/O TURN LANES	
LOG MILE	AREA (SY.)
19.112	225
18.495	225
18.770	225
18.375	225
18.184	225
MEDIAN LM 17.543 - 17.770 @ 30'	3995.20
TOTAL	5,120.20

SEE SHEET NO. 2B FOR PAVEMENT SCHEDULE

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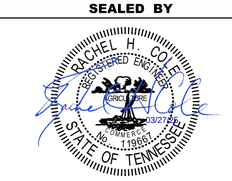

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	2C
		88S111-F8-002	
		88S111-F3-002	

GENERAL NOTES

GRADING

(1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

(3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

(3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

(1) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M.

FINAL PAVEMENT MARKING

(6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

(19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

(1) THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

(9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

(12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

(1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

(2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

(3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

(4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

(5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

(6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

(7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

(9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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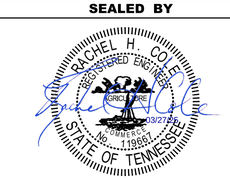

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	2D
		88S111-F8-002	
		88S111-F3-002	

SPECIAL NOTES

TRAFFIC CONTROL

(1) THE CONTRACTOR SHALL MAKE PROVISIONS TO SAFELY CONTROL TRAFFIC INGRESS AT ALL ROADWAY ENTRANCES WITHIN THE FLAGGED WORK ZONE TO THE SATISFACTION OF THE TDOT PROJECT ENGINEER, ALL COST ASSOCIATED WITH THE CONTROLS WILL BE INCLUDED IN THE PRICE BID FOR TRAFFIC CONTROL.

(2) EXISTING CONSTRUCTION, REGULATORY AND WARNING SIGNS WHICH CONFLICT WITH THE CONSTRUCTION SIGNING SHALL BE REMOVED DURING CONSTRUCTION AND REINSTALLED AS DIRECTED BY THE TDOT PROJECT ENGINEER. ALL COSTS TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-01, TRAFFIC CONTROL, PER LUMP SUM.

(3) THE CONTRACTOR SHALL GIVE THE TDOT PROJECT ENGINEER A MINIMUM OF SEVEN (7) DAYS NOTICE PRIOR TO STARTING WORK SO THAT SUFFICIENT NOTICE CAN BE PREPARED AND DISTRIBUTED TO THE MEDIA.

(4) IMMEDIATELY UPON COMPLETION OF EACH CONSTRUCTION PHASE, ALL TRAFFIC CONTROL ITEMS THAT ARE NOT NECESSARY FOR THE SUCCEEDING PHASE SHALL BE REMOVED, COVERED, OR TURNED TO FACE AWAY FROM THE TRAFFIC.

(5) THE CONTRACTOR SHALL PROVIDE FLAGGER AT EACH SIDEROAD WITHIN THE PAVING OPERATION.

(6) A MINIMUM OF TWO (2) TYPE “C” ARROW BOARDS WILL BE REQUIRED. IF THE CONTRACTOR SCHEDULES HIS OPERATIONS SUCH THAT MORE THAN TWO (2) ARROW BOARDS ARE REQUIRED. THE COST OF ALL ADDITIONAL ARROW BOARDS WILL BE AT THE CONTRACTOR'S EXPENSE.

(7) CHANGEABLE MESSAGE SIGNS UNITS (ITEM NO. 713-16.01) WILL BE REQUIRED ON THIS PROJECT. THE SIGNS WILL BE USED FOR ADVANCED WARNING OR AS DIRECTED BY THE ENGINEER.

(8) MESSAGE BOARDS SHALL BE DISPLAYED A MINIMUM OF SEVEN (7) DAYS PRIOR TO STARTING WORK TO INFORM PUBLIC OF UPCOMING CONSTRUCTION PROJECT.

(9) PRIOR TO DISPLAYING MESSAGES ON CHANGEABLE MESSAGE BOARDS, ALL MESSAGES SHALL BE APPROVED BY THE TDOT PROJECT ENGINEER AND REGION 2 TRAFFIC ENGINEER.

(10) INFORMATION ON CHANGEABLE MESSAGE BOARDS SHALL BE UPDATED TO REFLECT CURRENT CONSTRUCTION CONDITIONS ACTIVITIES AT ALL TIMES.

LANE CLOSURES

(1) THE RESTRICTION OF TRAFFIC TO ONE LANE SHALL NOT EXCEED ONE DAY'S PAVING OPERATION OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.

(2) THE CONTRACTOR SHALL KEEP ALL TRAFFIC LANES OPEN TO TRAFFIC DURING NON-WORKING HOURS AND/OR NON-WORK DAYS.

(3) OVERNIGHT LANE CLOSURES WILL NOT BE ALLOWED UNLESS DIRECTED BY THE TDOT PROJECT ENGINEER.

(4) THE CONTRACTOR SHALL NOT BE ALLOWED TO INTERRUPT TRAFFIC FLOW AND SHALL MAINTAIN ALL LANES OF TRAFFIC IN EACH DIRECTION ON THE FOLLOWING DAYS:

A. OFFICIAL STATE HOLIDAYS.

B. FRIDAY AT 6:00 PM UNTIL TUESDAY AT 7:00 PM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON MONDAY.

C. THURSDAY AT 6:00 PM UNTIL MONDAY AT 7:00 PM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON FRIDAY.

D. DURING LOCAL FESTIVALS, GAMES OR EVENTS THAT COULD BE IMPEDED BY THE PAVING OPERATIONS WHERE AND AS DIRECTED BY THE TDOT PROJECT ENGINEER.

PAVING

(1) ALL SURPLUS PATCHING AND JOINT MATERIAL IS TO BE REMOVED BEFORE PLACING TACK COAT AND SURFACE. THIS WORK WILL NOT BE MEASURED AND PAID FOR DIRECTLY BUT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION

(2) ANY QUANTITY REMAINING ON ITEMS COMPLETED PRIOR TO THE PAVING OPERATION WILL NOT BE CONVERTED TO ADDITIONAL ASPHALT FOR THE ROADWAY.

PAVEMENT MARKING

(1) THE CONTRACTOR IS RESPONSIBLE FOR THE LAYOUT OF ALL PAVEMENT MARKING. ANY CHANGE FROM THE EXISTING PAVEMENT MARKINGS MUST BE APPROVED BY THE REGION2 TRAFFIC MANAGER.

(2) ALL EXISTING PAVEMENT MARKING BETWEEN THE EDGE LINES (INCLUDING SPECIALTY PAVEMENT MARKINGS) SHALL BE REMOVED FLUSH WITH EXISTING SURFACE BEFORE APPLYING TACK COAT.

(3) NEW EDGE LINES SHALL BE RETRACED OVER THE EXISTING LINES ONCE THE MICRO-SURFACING AND HP FOG SEAL WORK IS COMPLETED USING 716-12.02

(4) EXISTING PAVEMENT MARKINGS IN THE SIDE ROAD RADII AND AREAS THAT ARE EXCLUDED FROM RECEIVING MICRO-SURFACE SHALL BE RE-TRACED AS DIRECTED BY THE TDOT PROJECT SUPERVISOR DURING FINAL STRIPING.

FOG SEALING SHOULDERS

(1) THE CONTRACTOR SHALL USE HIGH PERFORMANCE (HP) FOG SEAL FROM QUALIFIED PRODUCTS LIST QPL.40.001.

(2) FINAL EDGE LINE STRIPING SHALL NOT BE APPLIED UNTIL HP FOG SEAL APPLICATION HAS BEEN COMPLETED.

(3) THE CONTRACTOR SHALL USE EXTREME CAUTION WHEN SPRAYING FOG SEAL NEXT TO THE BARRIER WALL TO PREVENT SPRAYING ON BARRIER WALL. ENSURE FOG SEAL DOES NOT ENTER DRAINAGE STRUCTURES.

MISCELLANEOUS

(1) ALL SAFETY IMPROVEMENTS TO BE PAID FOR UNDER PROJECT NUMBERS: NH/HSIP-111(129), 88S111-F3-002.

ENVIRONMENTAL

(1) THE CONTRACTOR SHALL USE ANY MEASURE NECESSARY TO ENSURE THAT NO CONSTRUCTION ACTIVITY OR CONSTRUCTION EQUIPMENT WILL ENTER ANY PORTION OF STR-1 AND THAT THE STREAM AND SURROUNDING VEGETATION WILL NOT BE DISTURBED AND ARE PROTECTED FROM SEDIMENT AND OTHER POLLUTANTS.

(2) SPECIAL NOTE TO CONTRACTOR: CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO ENSURE THAT NO DEBRIS IS DROPPED INTO THE STREAM. THIS SHALL BE ACCOMPLISHED BY THE USE OF BASKETS, NETTING, WRAPPING, WORK PLATFORM, OR OTHER SIMILARLY EFFECTIVE MEANS. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL NOT BE ALLOWED TO ENTER THE STREAM AND SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. COST OF REMOVING AND DISPOSING OF DEBRIS SHALL BE INCLUDED IN ITEMS BID ON.

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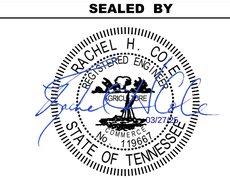

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	2E
		88S111-F8-002	
		88S111-F3-002	

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

(4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.

(9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

(11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).

(12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

(15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

(1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

SCOPE OF WORK

(1) BRIDGE REPAIR, MICRO-SURFACING, FOG SEALING, AND PAVEMENT MARKINGS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

(1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

(6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

(8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

(29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.

(30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.

(31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.

(32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.

(33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.

(34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

(35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.

(36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.

(37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.

(38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.

(39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.

(40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

(44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.

(45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.

(46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.

(47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.

(48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.

(49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.

(50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.

(51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

(52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

(53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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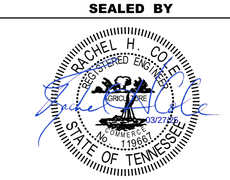

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	2F
		88S111-F8-002	
		88S111-F3-002	

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)

M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES L	x	W	S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
G20-1	ROAD WORK NEXT 5 MILES	48"	x	24"	8	3	24
G20-2	END ROAD WORK	48"	x	24"	8	14	112
W4-2L	LANE ENDS (LT)	48"	x	48"	16	4	64
W4-2R	LANE ENDS (RT)	48"	x	48"	16	4	64
W8-7	LOOSE GRAVEL	48"	x	48"	16	7	112
W20-1	ROAD WORK AHEAD	48"	x	48"	16	11	176
W20-1	ROAD WORK 1 MILE	48"	x	48"	16	3	48
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16	3	48
W20-1	ROAD WORK 1000 FT	48"	x	48"	16	3	48
W20-4	ONE LANE ROAD AHEAD	48"	x	48"	16	2	32
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x	48"	16	4	64
W20-5L	LEFT LANE CLOSED 1500 FEET	48"	x	48"	16	4	64
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x	48"	16	4	64
W20-5R	RIGHT LANE CLOSED 1500 FEET	48"	x	48"	16	4	64
W20-7	FLAGGER (SYMBOL)	48"	x	48"	16	2	32
W21-2	FRESH OIL	48"	x	48"	16	3	48
						TOTAL	1064

* SEE FIGURE 6F-1 OF FIGURES SHOWN IN CURRENT M.U.T.C.D. THIS CONSTRUCTION SIGNING IS TO BE CONSTRUCTED AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.

PROPOSED GUARDRAIL DELINEATION ENHANCEMENT

LOCATION	LM TO LM	COLOR	TOTAL (EA)
NB RT	15.082 - 15.301	WHITE	193
NB RT	15.655 - 15.809	WHITE	136
NB RT	15.911 - 15.977	WHITE	58
NB RT	16.092 - 16.371	WHITE	246
NB RT	16.384 - 16.926	WHITE	477
NB RT	17.360 - 17.775	WHITE	365
NB RT	18.277 - 18.337	WHITE	53
NB RT	18.693 - 18.724	WHITE	27
NB RT	19.021 - 19.141	WHITE	106
NB RT	19.285 - 19.419	WHITE	118
NB RT	19.457 - 19.501	WHITE	39
NB LT	18.465 - 19.503	YELLOW	913
SB RT	19.503 - 19.464	WHITE	34
SB RT	18.341 - 18.262	WHITE	70
SB RT	17.874 - 17.652	WHITE	195
SB RT	17.221 - 17.182	WHITE	34
SB RT	16.361 - 16.072	WHITE	254
SB RT	15.821 - 15.463	WHITE	315
SB RT	15.315 - 15.068	WHITE	217
GUARDRAIL DELINEATION ENHANCEMENT ITEM NO. 705-04.20		TOTAL	3850

DELINEATORS SHALL BE:
- "HINGED-TYPE" DELINEATORS FROM TDOT'S CURRENT QPL.
- MOUNTED TO ALL GUARDRAIL POSTS EXCEPT END TERMINAL SECTIONS.
- ATTACHED TO GUARDRAIL POST AS PER MANUFACTURER'S INSTRUCTION.
- USE MANUFACTURER'S RECOMMENDED ADHESIVE.

BRIDGE DECK RECOMMENDATIONS (RESURFACING)

BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
88SR1110007	19.51 R	CANEY FORK RIVER	517'-6"	SEAL WITH TYPE 1 THIN EPOXY OVERLAY, REPLACE BRIDGE JOINTS SEE BRIDGE SHEETS FOR MORE INFORMATION.
88SR1110008	19.51 L	CANEY FORK RIVER	486'-10"	SEAL WITH TYPE 1 THIN EPOXY OVERLAY, REPLACE BRIDGE JOINTS SEE BRIDGE SHEETS FOR MORE INFORMATION.

UTILITY ADJUSTMENTS (RESURFACING)

THERE ARE NO UTILITY ADJUSTMENTS WITHIN PROJECT LIMITS

STORM DRAIN ADJ. (RESURFACING)

THERE ARE NO STORM DRAIN ADJUSTMENTS WITHIN PROJECT LIMITS

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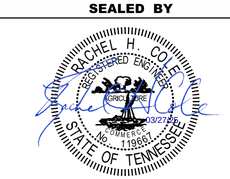

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED QUANTITIES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	3
		88S111-F8-002	
		88S111-F3-002	

UTILITY

(1) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER “THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT”, THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

(2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.

(3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED ‘AROUND’ UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR’S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.

(5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

CATV:
BLEDSOE TELEPHONE CO-OP/CATV
PO BOX 609, 338 CUMBERLAND AVENUE
PIKEVILLE, TN 37367
CONTACT: MATT BOYNTON
OFFICE PHONE: 423 447 2121
Email: mboynton@bledsoe.net

ELECTRIC:
CANEY FORK ELECTRIC COOPERATIVE, INC.
PO BOX 272, 920 SMITHVILLE HIGHWAY
MCMINNVILLE, TN 37110-1654
CONTACT: MICHAEL MILLRANEY
OFFICE PHONE: 931 414 6730
Email: mmillraney@caneyforkec.com

GAS:
MIDDLE TENNESSEE NATURAL GAS
PO BOX 670, 1030 WEST BROAD STREET
SMITHVILLE, TN 37166
CONTACT: MATT STENNETT
OFFICE PHONE: 931 754 3515
Email: mstennett@mtng.com

TELEPHONE:
BEN LOMAND CONNECT
PO BOX 670, 311 NORTH CHANCERY STREET
MCMINNVILLE, TN 37110
CONTACT: RICHARD BOYD
OFFICE PHONE: 931 668 6692
Email: richardboyd@benlomand.org

WATER:
FALL CREEK FALLS UTILITY DISTRICT
27364 STATE ROUTE 30
PIKEVILLE, TN 37367
CONTACT: ROGER WOODEN
OFFICE PHONE: 423 881 5065
Email: fcfud@bledsoe.net

CATV / FIBER OPTICS:
CHARTER COMMUNICATIONS
851 S. WILLOW AVENUE, SUITE 206
COOKEVILLE, TN 38501
CONTACT: MORGAN WILCHER
OFFICE PHONE: 931 239 9222
Email: Morgan.Wilcher@charter.com

SEWER:
CITY OF SPENCER
PO BOX 187, 12180 STATE ROUTE 111
SPENCER, TN 38585
CONTACT: MAYOR ALISA FARMER
OFFICE PHONE: 931 946 2351
Email: spencermayor@benlomand.net

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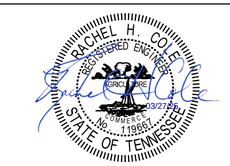

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND
UTILITY OWNERS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-111(129)	T1
		88S111-F8-002	
		88S111-F3-002	

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

   a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

   b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

   c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

   d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

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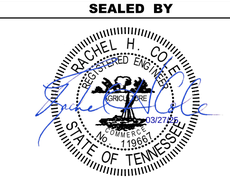

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

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THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Brian Egli
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PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	STRUCTURE-SIGN 1
BRIDGE PLANS	B1 THRU B9

YEAR	PROJECT NO.	SHEET NO.
2025	88S111-M3-002	STRUCTURE-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

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FED. PROJ. NO. NH/HSIP-111(129)

PIN 134123.00

PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B1

REVISIONS

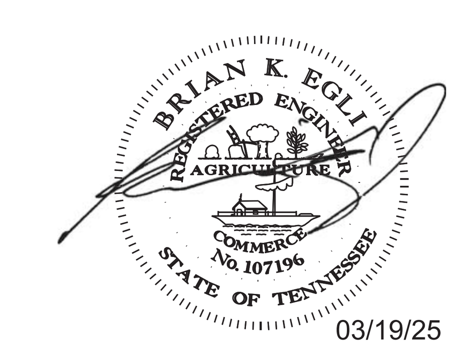
NO.	DATE	BY	BRIEF DESCRIPTION

INDEX OF DRAWINGS

INDEX OF DRAWINGS	DWG. NO.	LAST REV. DATE
SIGNATURE SHEET	STRUCTURE-SIGN 1	
INDEX OF DRAWINGS	B1	
BRIDGE TABULATION, ESTIMATED QUANTITIES, AND EXPANSION JOINT REPAIR NOTES	B2	
TYPE I THIN EPOXY OVERLAY NOTES	B3	
APPROACH SLAB REPAIR DETAIL NOTES	B4	
BRIDGE DECK REPAIR NOTES	B5	
PLAN VIEW (88SR1110007)	B6	
PHASE CONSTRUCTION	B7	
PLAN VIEW (88SR1110008)	B8	
PHASE CONSTRUCTION	B9	

INDEX OF REFERENCE DRAWINGS

INDEX OF REFERENCE DRAWINGS	DWG. NO.
LAYOUT	M-331-66
SUPERSTRUCTURE	M-331-69
LAYOUT	M-331-83
SUPERSTRUCTURE	M-331-87



03/19/25

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

INDEX OF DRAWINGS
88-SR111-19.51R&L
OVER
CANEY FORK RIVER
BR. NOS. 88SR1110007
88SR1110008
VAN BUREN COUNTY
2025

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	6/24
SUPERVISED BY	K. MARTINKO	DATE	6/24
CHECKED BY		DATE	

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PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B2

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES

LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.44 EXPANSION JOINT REPAIR L.F.	604-10.53 CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) S.Y.	617-04.01 TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
88-SR111-19.51R OVER CANEY FORK RIVER (88SR1110007)	M-331-66 M-331-69	EXPANSION JOINT REPAIR CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	87	5	5	2545
88-SR111-19.51L OVER CANEY FORK RIVER (88SR1110008)	M-331-83 M-331-87	EXPANSION JOINT REPAIR CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	81	5	7	2407
TOTAL			168	10	12	4952

EXPANSION JOINT REPAIR NOTES

THE JOINT HEADER SYSTEM SHALL BE FROM QPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM QPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM QPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM QPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE QPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF ¾ OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE
03/19/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION, ESTIMATED QUANTITIES, AND EXPANSION JOINT REPAIR NOTES
88-SR111-19.51R&L
OVER
CANEY FORK RIVER
BR. NOS. 88SR1110007
88SR1110008
VAN BUREN COUNTY
2025

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	6/24
SUPERVISED BY	K. MARTINKO	DATE	6/24
CHECKED BY		DATE	

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PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B3

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IN NOT ACHIEVED, A RETEST SHAL BE PERFORMED IN ADJOINING AREAS. SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

** SPECIAL NOTE:

THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACKCOAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY.

MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACKCOAT OR DEBRIS REMOVAL.

REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE & COMMERCE
NO. 107196
STATE OF TENNESSEE
03/19/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPE I THIN EPOXY
OVERLAY NOTES
88-SR111-19.51R&L
OVER
CANEY FORK RIVER
BR. NOS. 88SR1110007
88SR1110008
VAN BUREN COUNTY
2025

B3

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	6/24
SUPERVISED BY	K. MARTINKO	DATE	6/24
CHECKED BY		DATE	

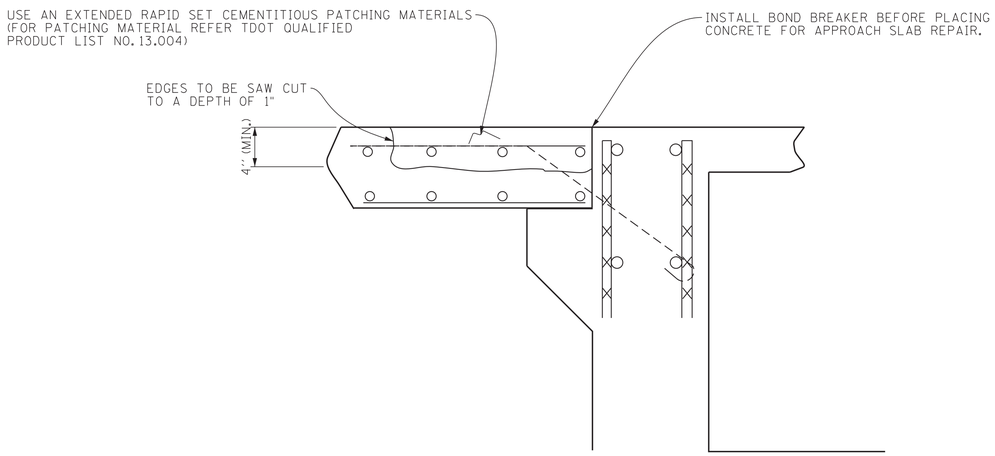
$$$$SYTIME$$$$$$
260124 PM
$$$$DGNSPEC$$$$$$

PIN 134123.00

PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B4

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



CONCRETE APPROACH PAVEMENT REPAIR DETAILS (STRUCTUAL REPAIR)

NOTE: REMOVE CONCRETE TO A DEPTH OF ¾" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR (S.Y.).

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT.) THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

(2) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

BRIDGE DECK REPAIR MATERIAL:

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDOT QUALIFIED PRODUCTS LIST:
13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS.
MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

APPROACH SLAB REPAIR
DETAIL NOTES
88-SR111-19.51R&L
OVER
CANEY FORK RIVER
BR. NOS. 88SR1110007
88SR1110008
VAN BUREN COUNTY
2025

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	6/24
SUPERVISED BY	K. MARTINKO	DATE	6/24
CHECKED BY		DATE	

$$$$SYTIME$$$$$$
26D47 PM
$$$$DGNSPEC$$$$

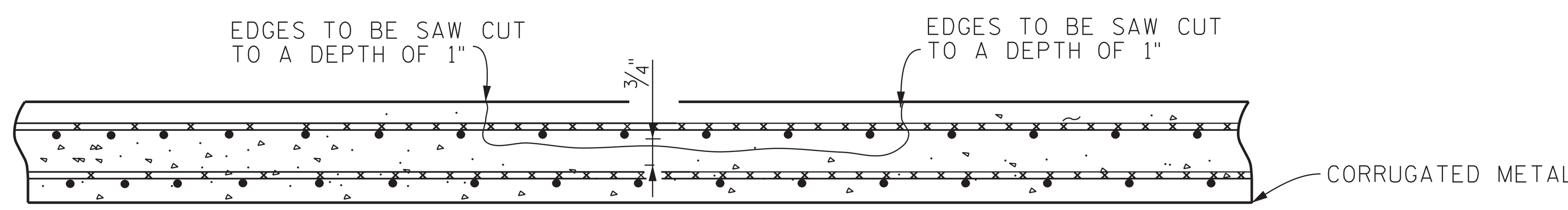
PIN 134123.00

PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B5

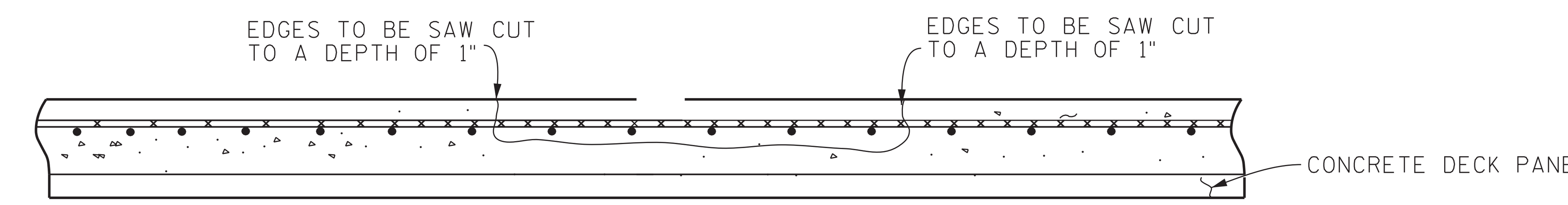
REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

USE AN EXTENDED RAPID SET CEMENTITIOUS PATCHING MATERIALS (FOR PATCHING MATERIAL REFER TDOT QUALIFIED PRODUCT LIST NO. 13.004)



88SR1110008



88SR1110007

SKETCH SHOWING DECK REPAIR (STRUCTURAL REPAIR):

NOTE: PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF ¾" BELOW THE BOTTOM BAR OF THE TOP MAT OF REINFORCING STEEL.

NOTE: PARTIAL DEPTH DECK REPAIRS:
ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBARS AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

NOTE: ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 lb. CLASS SHALL NOT BE USED. TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

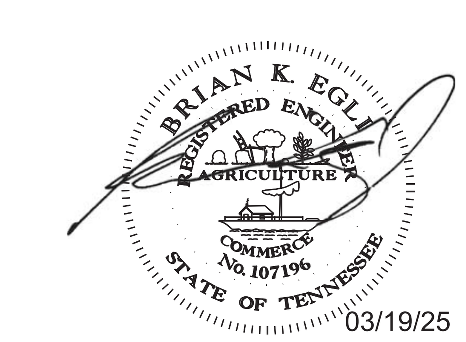
(2) CHIPPING HAMMERS OF THE 15 lb. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

BRIDGE DECK REPAIR MATERIAL:

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDOT QUALIFIED PRODUCTS LIST:
13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS.
MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.
MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	6/24
SUPERVISED BY	K. MARTINKO	DATE	6/24
CHECKED BY		DATE	

$$$$SYTIME$$$$$$
2:01:11 PM
$$$$DGNSPEC$$$$



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

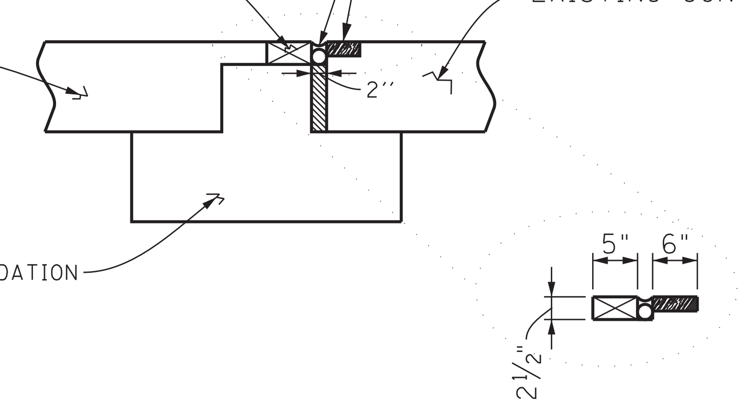
BRIDGE DECK
REPAIR NOTES
88-SR111-19.51R&L
OVER
CANEY FORK RIVER
BR. NOS. 88SR1110007
88SR1110008
VAN BUREN COUNTY
2025

PIN 134123.00

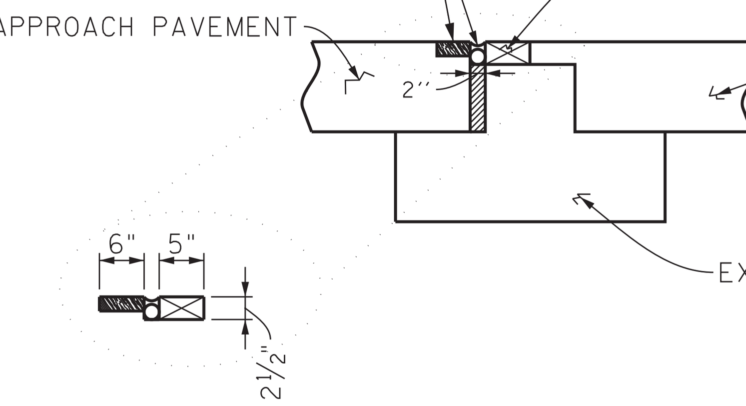
PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B6

REVISIONS

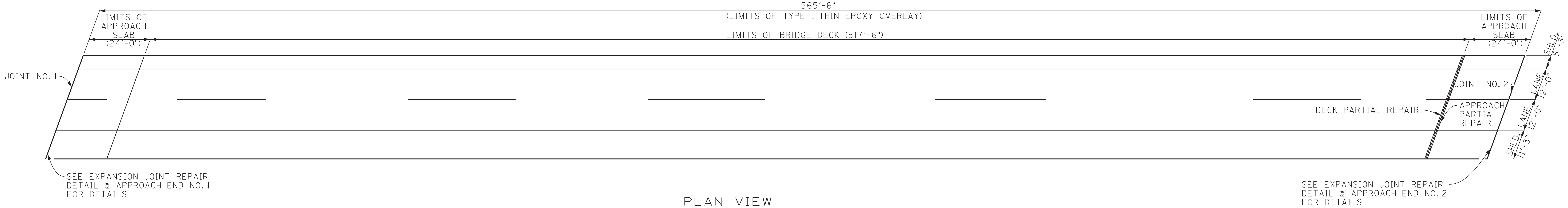
NO.	DATE	BY	BRIEF DESCRIPTION



EXPANSION JOINT REPAIR DETAIL @ APPROACH END NO. 1

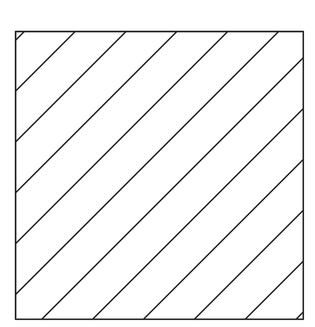


EXPANSION JOINT REPAIR DETAIL @ APPROACH END NO. 2

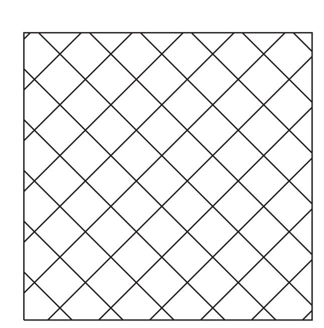


PLAN VIEW

BRIDGE ID. NO. 88SR1110007



AREAS OF DECK PARTIAL REPAIR
SEE PAGE B5 FOR REPAIR DETAIL



AREAS OF APPROACH PARTIAL REPAIR
SEE PAGE B4 FOR REPAIR DETAIL

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	6/24
SUPERVISED BY	K. MARTINKO	DATE	6/24
CHECKED BY		DATE	

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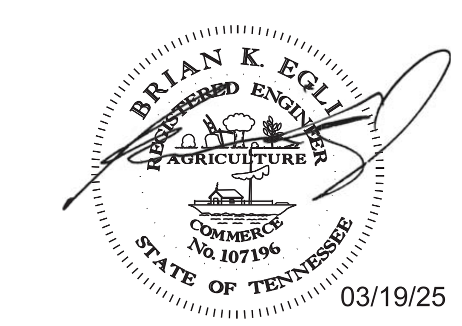
2:01:34 PM
$$$$DGNSPEC$$$$



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

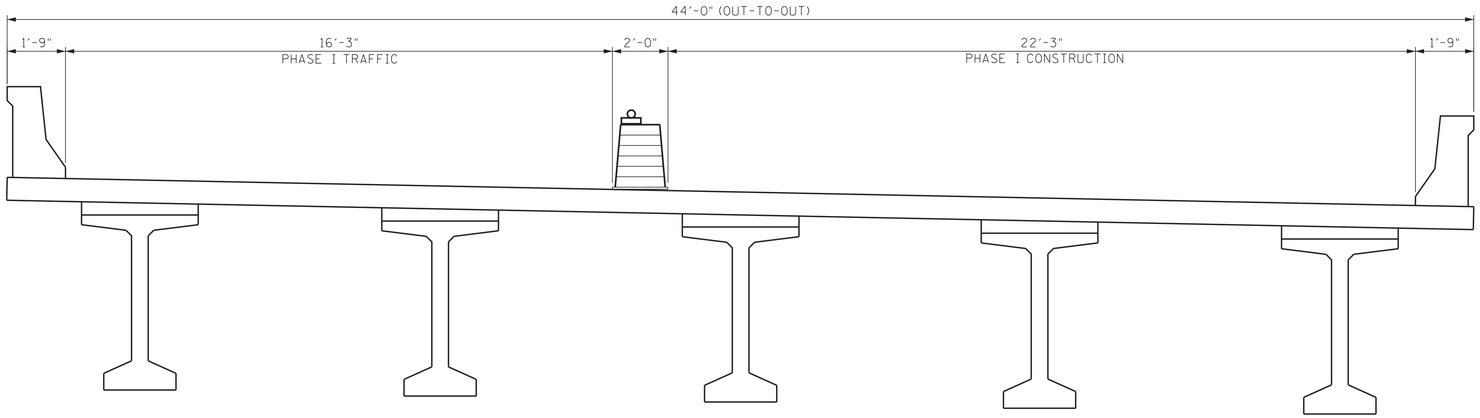
PLAN VIEW
88-SR111-19.51R
OVER
CANEY FORK RIVER
BR. NO. 88SR1110007
VAN BUREN COUNTY
2025

PIN 134123.00

PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B7

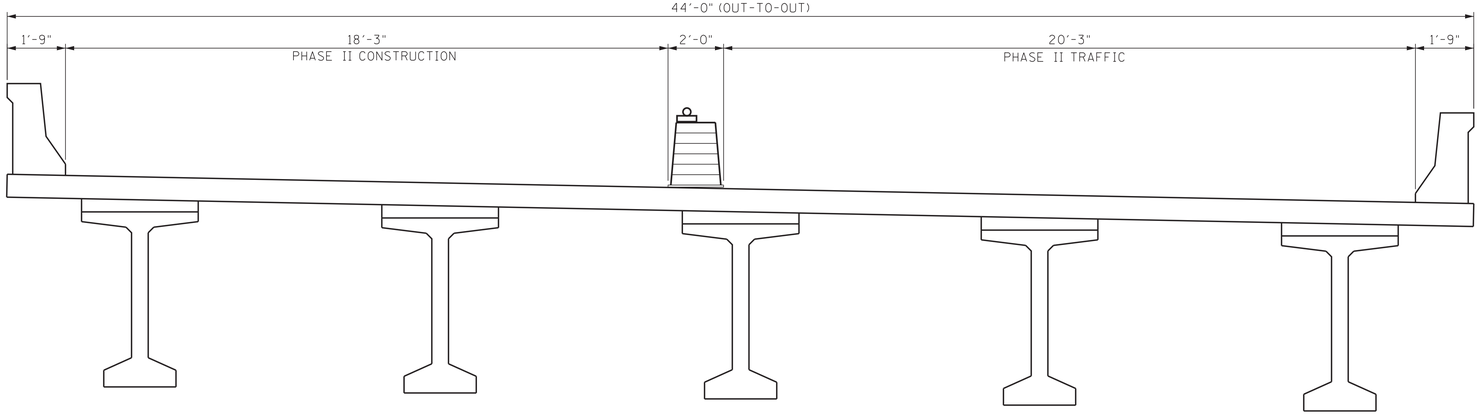
REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



NORTHBOUND BRIDGE
(88-SR111-19.51R)

PHASE I CONSTRUCTION
(LOOKING AHEAD ON SURVEY)



NORTHBOUND BRIDGE
(88-SR111-19.51R)

PHASE II CONSTRUCTION
(LOOKING AHEAD ON SURVEY)

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE & COMMERCE
No. 107196
STATE OF TENNESSEE
03/19/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
88-SR111-19.51R
OVER
CANEY FORK RIVER
BR. NO. 88SR1110007
VAN BUREN COUNTY
2025

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	6/24
SUPERVISED BY	K. MARTINKO	DATE	6/24
CHECKED BY		DATE	

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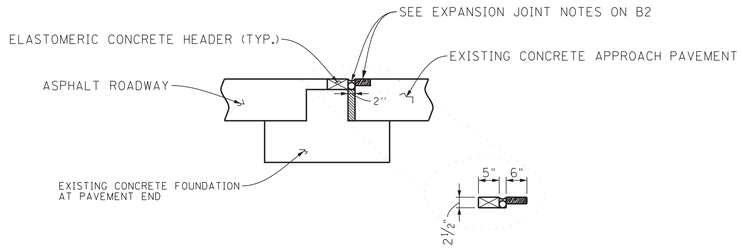
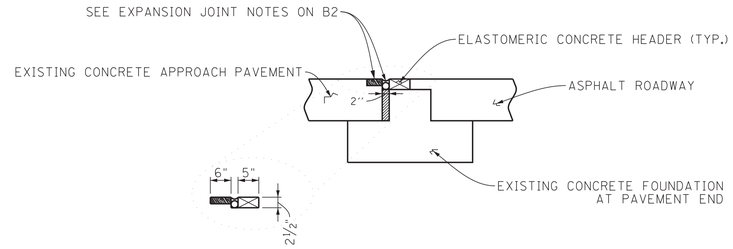


EXPANSION JOINT REPAIR DETAIL
@ APPROACH END NO. 1



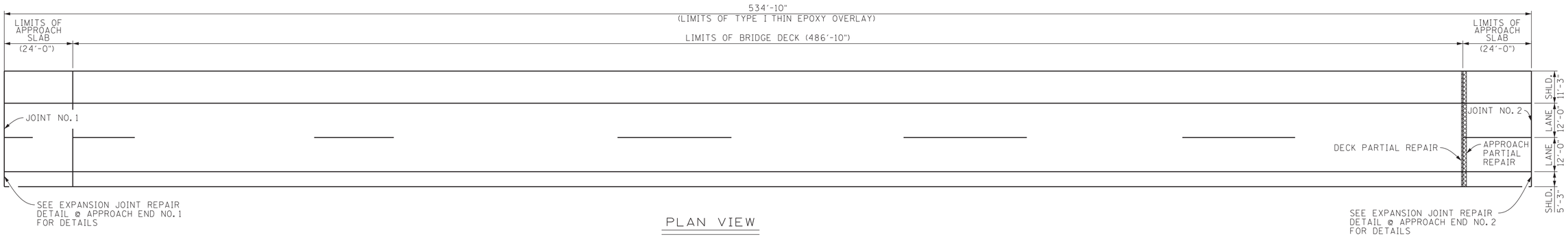
EXPANSION JOINT REPAIR DETAIL
@ APPROACH END NO. 2

PIN 134123.00

PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B8

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



PLAN VIEW

BRIDGE ID. NO. 88SR1110008

AREAS OF DECK PARTIAL REPAIR
SEE PAGE B5 FOR REPAIR DETAIL

AREAS OF APPROACH PARTIAL REPAIR
SEE PAGE B4 FOR REPAIR DETAIL

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE & COMMERCE
No. 107196
STATE OF TENNESSEE
03/19/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
88-SR111-19.51L
OVER
CANEY FORK RIVER
BR. NO. 88SR1110008
VAN BUREN COUNTY
2025

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	6/24
SUPERVISED BY	K. MARTINKO	DATE	6/24
CHECKED BY		DATE	

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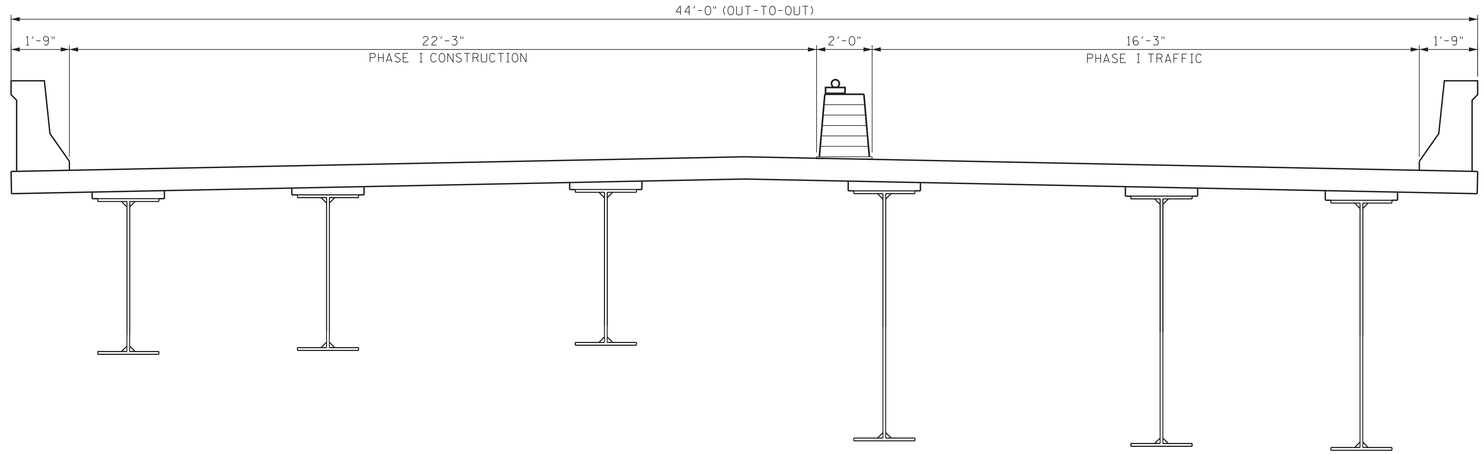
PIN 134123.00

PROJECT NO.	YEAR	SHEET NO.
88S111-M3-002	2025	B9

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

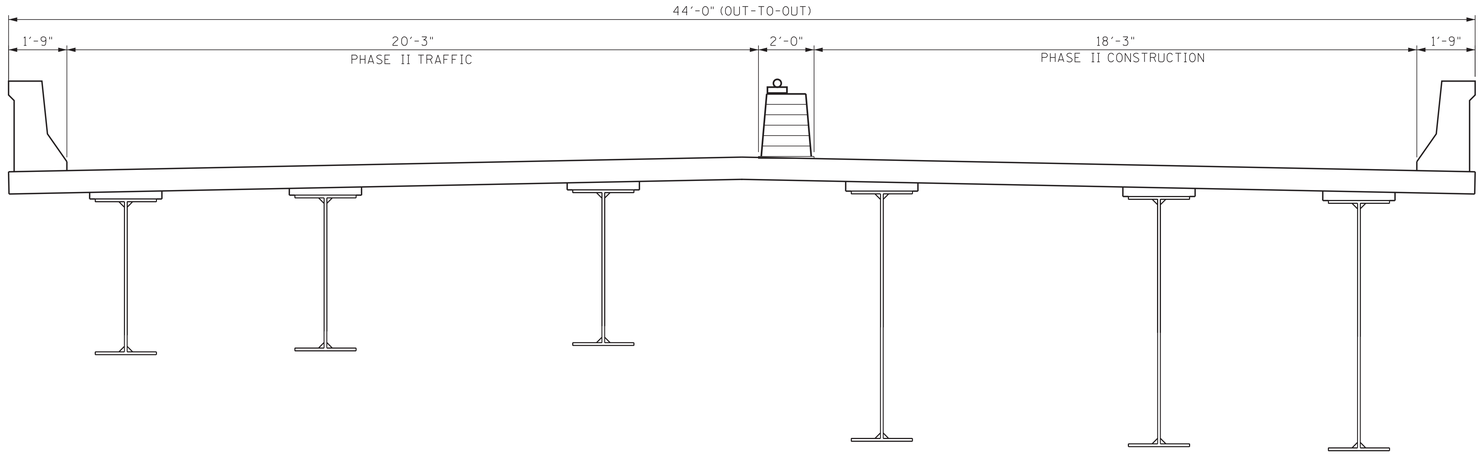


SOUTHBOUND BRIDGE

(88-SR111-19.51L)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)



SOUTHBOUND BRIDGE

(88-SR111-19.51L)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

03/19/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
88-SR111-19.51L
OVER
CANEY FORK RIVER
BR. NO. 88SR1110008
VAN BUREN COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 6/24
SUPERVISED BY K. MARTINKO DATE 6/24
CHECKED BY _____ DATE _____

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26243 PM
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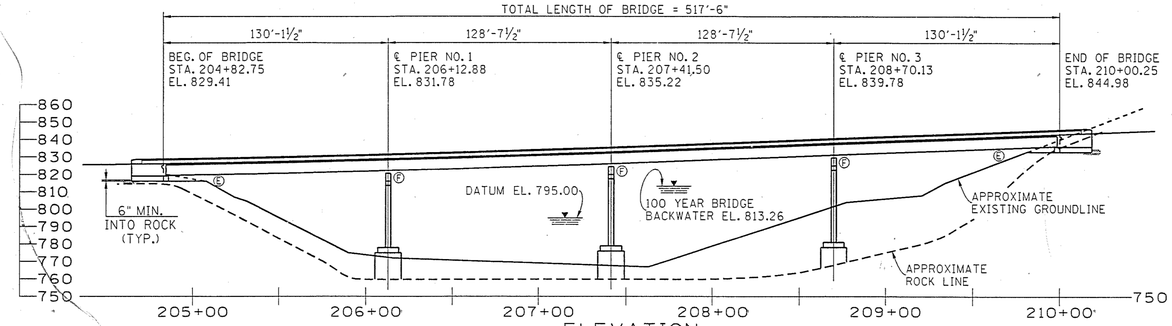
LAY043F.DGN

CONST. NO. 88027-3227-64
93003-3217-64

PROJECT NO.	YEAR	SHEET NO.
APD-NH-111(29)	1996	

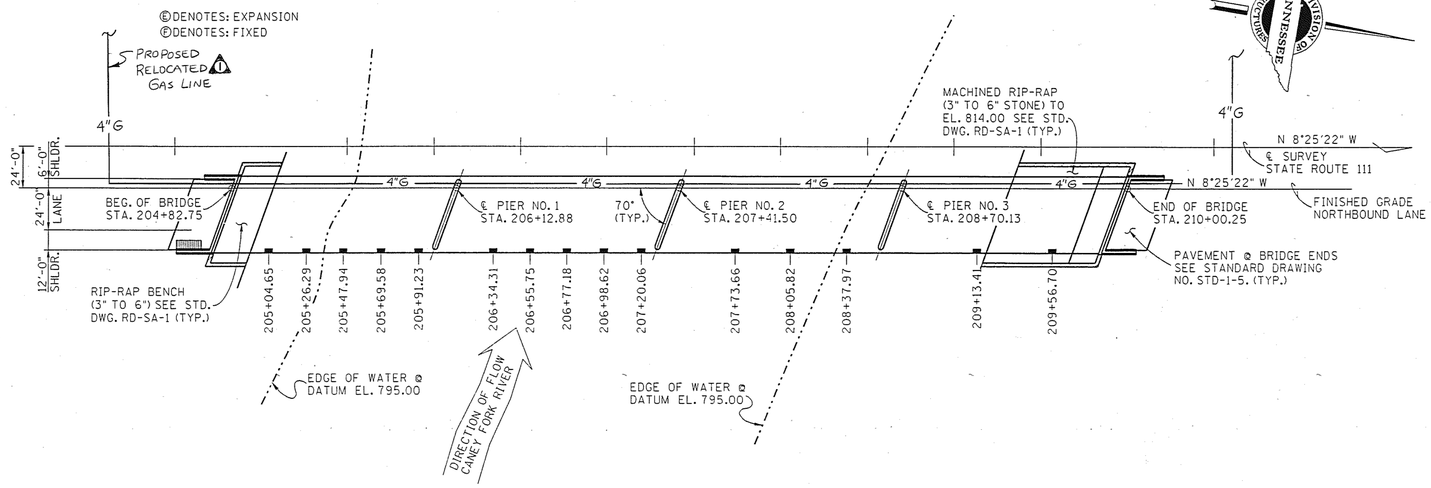
REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	7-16-96	MEF	ADDED PROPOSED RELOCATED GAS LINE AND LAST REV. DATES

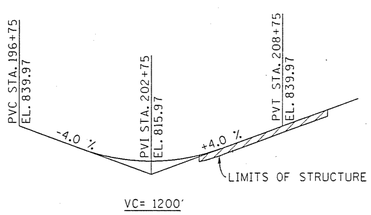


ELEVATION
(NORTHBOUND LANE)
(SCALE: 1" = 40')

Ⓔ DENOTES: EXPANSION
Ⓕ DENOTES: FIXED



PLAN
(SCALE: 1" = 40')
DENOTES: END OF BRIDGE DRAIN REQUIRED. SEE STD. DWG. NO. STD-1-6, 7, & 9.
DENOTES: BRIDGE DECK DRAIN REQUIRED. SEE STD. DWG. NO. STD-1-2.



GRADE SKETCH
(NORTHBOUND LANE)

HYDRAULIC DATA

DRAINAGE AREA = 518 sq. mi.
DESIGN DISCHARGE (100 YR.) = 39,900 cfs.
WATER AREA PROVIDED BELOW EL. 813.26 = 10,760 sq. ft.
100 YEAR VELOCITY = 3.71 ft./sec.
100 YEAR BRIDGE BACKWATER = 0.0 ft. AT EL. 813.26
ROADWAY OVERTOPPING EL. = 825.97
500 YEAR DISCHARGE = 51,260 cfs AT EL. 818.57

LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT	M-331-66	7-16-96
GENERAL NOTES & ESTIMATED QUANTITIES	M-331-67	7-16-96
FOUNDATION DATA	M-331-68	
SUPERSTRUCTURE	M-331-69	
SUPERSTRUCTURE DETAILS	M-331-70	
SUPERSTRUCTURE DETAILS	M-331-71	
PRESTRESSED BULB-TEE BEAM DETAILS	M-331-72	
ABUTMENT NO. 1	M-331-73	7-16-96
ABUTMENT NO. 2	M-331-74	7-16-96
ABUTMENT NO. 1 & 2 DETAILS	M-331-75	
ABUTMENT NO. 1 & 2 DETAILS	M-331-76	7-16-96
ABUTMENT NO. 1 & 2 DETAILS	M-331-77	
PIER NO'S. 1-3	M-331-78	
PIER NO'S. 1-3 DETAILS	M-331-79	
FINAL FOUNDATION DATA	M-331-80	
BILL OF STEEL	M-331-81	7-16-96
BILL OF STEEL	M-331-82	

LIST OF STANDARD DRAWINGS

	DWG. NO.	LAST REV. DATE
* BRIDGE RAILING CONCRETE PARAPET	STD-1-1	12-18-95
* SLIDER PLATES AND DECK DRAINS	STD-1-2	9-11-95
* REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-5	12-18-95
BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-6	5-1-95
BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-7	5-1-95
BRIDGE END DRAIN DETAILS 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-9	5-1-95
STRIP SEAL EXPANSION JOINT NEW CONSTRUCTION	STD-3-1	2-14-94
STRIP SEAL EXPANSION JOINT NEW CONSTRUCTION	STD-3-2	2-14-94
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-1	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-2	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-3	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STD-4-4	6-10-96
STANDARD SEISMIC DETAILS	STD-6-1	5-01-95
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STD-10-1	5-11-92
STD. DETAILS AND INT. DIAPH. DETAILS FOR BULB-TEE BEAMS	STD-14-1	2-14-95
SLOPE PROTECTION	RD-SA-1	4-29-88

LIST OF SPECIAL PROVISIONS

	PROV. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	3-06-95
COFFERDAMS AND FOUNDATION SEALS	604F	8-2-96
STRIP SEAL EXPANSION JOINTS	604S	2-19-96

* DENOTES: THESE DRAWINGS TO BE PRINTED WITH PLANS.

2016 ADT = 4,810
42'-0" ROADWAY W/STD-1-1 PARAPET
DESIGN SPEED = 60 MPH

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
NORTHBOUND LANE
LAYOUT
STATE ROUTE 111
OVER CANEY FORK RIVER
BRIDGE ID #88SR1110007
STATION 207+41.50
VAN BUREN - WHITE COUNTIES
1996

ROBERT M. DISHMAN REGISTERED ENGINEER CIVIL No. 575A STATE OF TENNESSEE

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY M. FLOWERS (PDD) DATE 11-95
DRAWN BY K.L.F. & C.A. BERNATEK DATE 1-96
SUPERVISED BY J. FIELDS/G. HALL DATE 1-96
CHECKED BY M. FLOWERS DATE 6-96

M-331-66

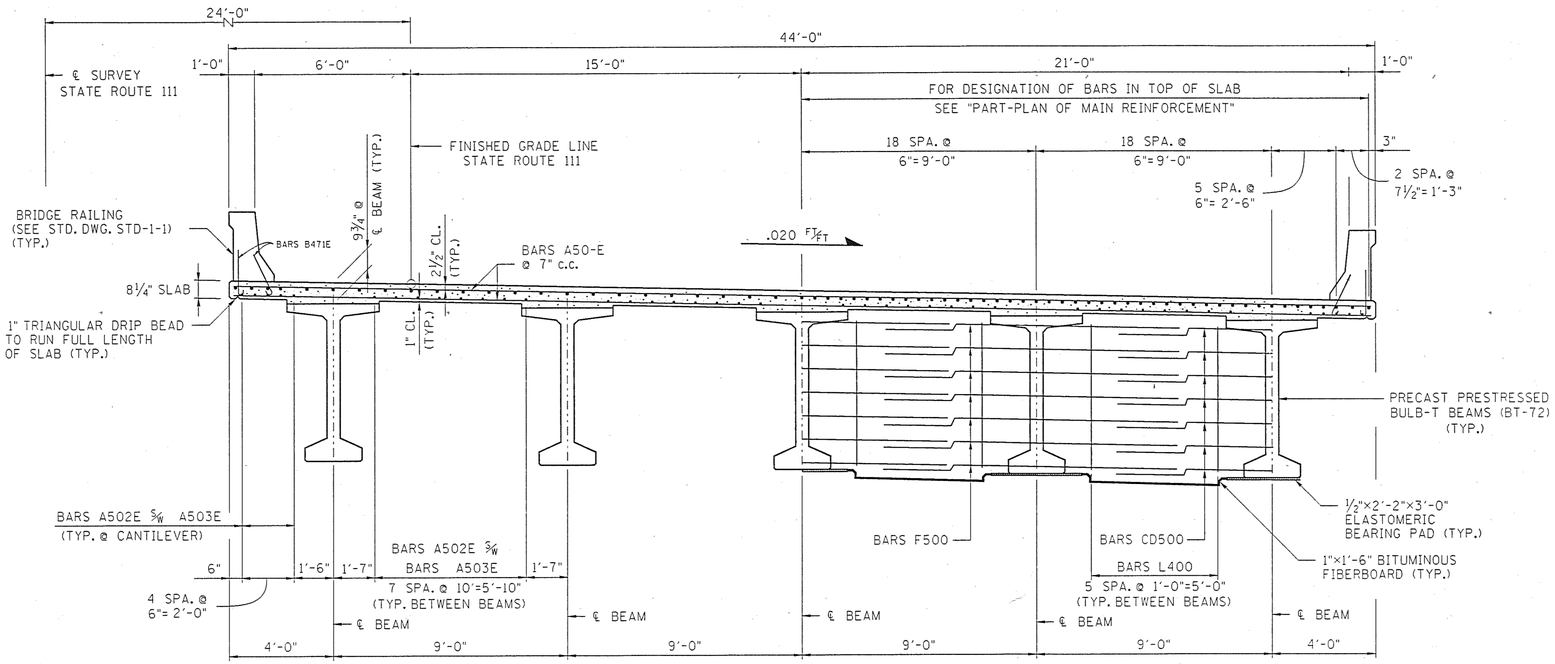
SP093F.DGN

CONST. NO. 88027-3227-64
93003-3217-64

PROJECT NO.	YEAR	SHEET NO.
APD-NH-111(29)	1996	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



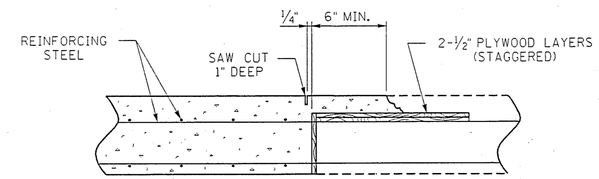
HALF-SECTION NEAR MIDSPAN

HALF-SECTION NEAR BENT

TYPICAL CROSS SECTION

(LOOKING AHEAD ON SURVEY)

NOTE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION EXCEPT NO JOINT MAY BE LOCATED CLOSER THAN 1/5 SPAN LENGTH FROM AN INTERIOR SUPPORT. THE SLAB OVER AN INTERIOR SUPPORT CAN ONLY BE POURED WHEN THE SLAB IN THE MIDDLE HALF OF THE SPANS ON EACH SIDE OF THE SUPPORT EITHER HAVE BEEN COMPLETED OR ARE POURED CONCURRENTLY. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT THE EXTERIOR BEAM FROM TWISTING. NO EQUIPMENT SHALL BE PERMITTED ON THE BRIDGE UNTIL ALL POURS ARE MADE AND THE CONCRETE IS PROPERLY CURED. ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN BELOW.

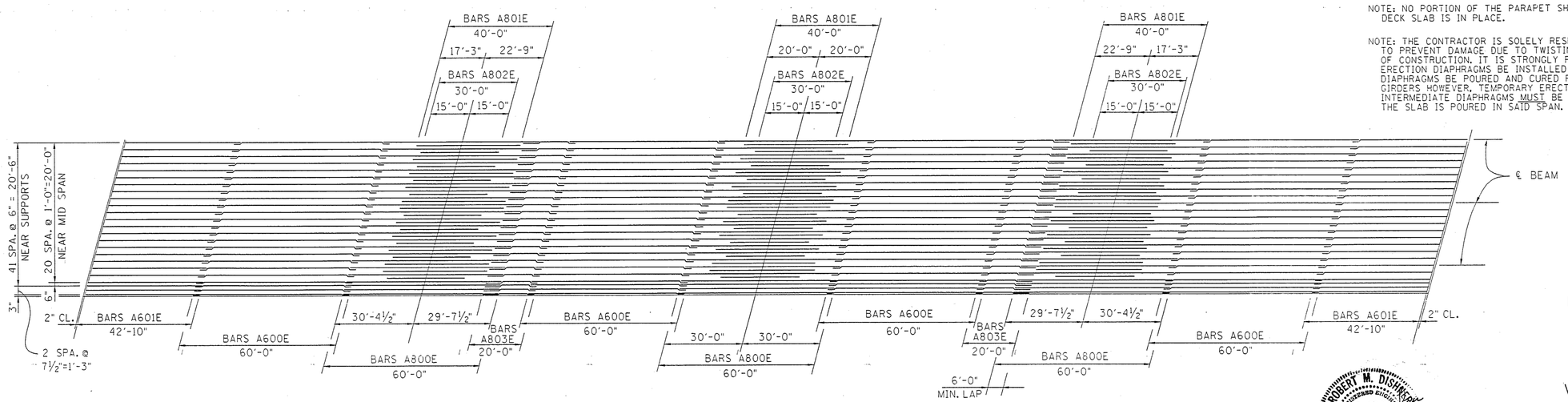


SLAB CONSTRUCTION JOINT DETAIL

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR BRIDGE RAIL. THE BRIDGE RAIL SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. (ALSO SEE DRAWING NO. STD-1-1.)

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.



HALF PLAN OF MAIN REINFORCEMENT

ESTIMATED QUANTITIES		
CLASS "D" CONCRETE (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.	REINFORCING STEEL LB.
668	169.477	3.103

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
NORTHBOUND LANE
SUPERSTRUCTURE
STATE ROUTE 111 OVER
CANEY FORK RIVER
STATION 207+41.50
VAN BUREN - WHITE COUNTIES
1996

ROBERT M. DISHMAN
REGISTERED ENGINEER
No. 5754
STATE OF TENNESSEE

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY	M. FLOWERS	DATE	1/96
DRAWN BY	JERRY BENSON	DATE	1/96
SUPERVISED BY	J. FIELDS/G. HALL	DATE	1/96
CHECKED BY	M. FLOWERS	DATE	6/96

M-331-69

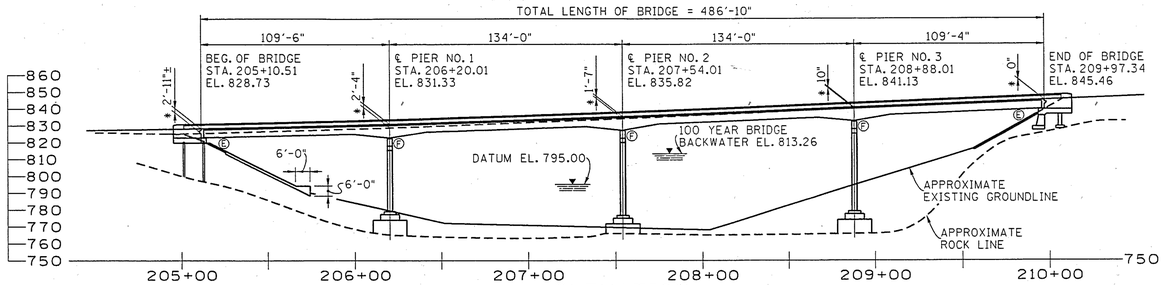
LAY043F.DGN

CONST. NO. 88027-3227-64
93003-3217-64

PROJECT NO.	YEAR	SHEET NO.
APD-NH-111(29)	1996	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	7-12-96	M.E.F.	ADDED GAS LINE AND NOTES & LAST REV. DATES
2	3-31-97	M.E.F.	ADDED GAS LINE RELOCATION NOTE
3	1-15-98	M.E.F.	LATEST REVISION DATE



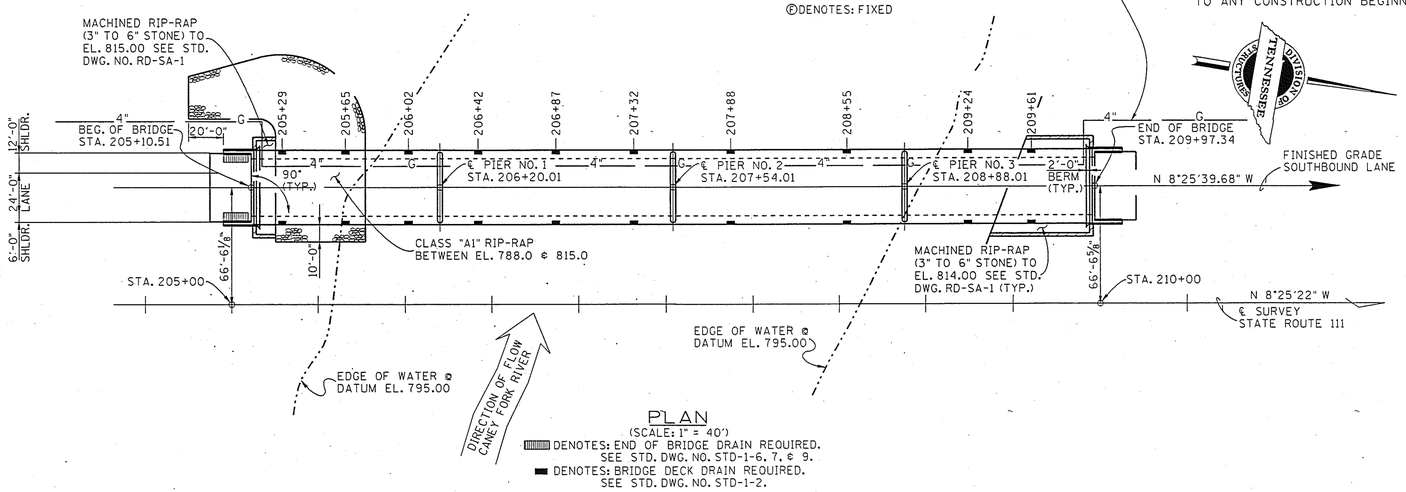
ELEVATION
(SOUTHBOUND LANE)
(SCALE: 1" = 40')

*APPROXIMATE DISTANCE THAT EXISTING STRUCTURE IS TO BE JACKED.

NOTE: EXISTING 4" GAS LINE TO BE RELOCATED ON NBL STRUCTURE.

EXISTING 4" GAS LINE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE THE REMOVAL AND RELOCATION OF THE GAS LINE WITH THE UTILITY PRIOR TO ANY CONSTRUCTION BEGINNING.

(E) DENOTES: EXPANSION
(F) DENOTES: FIXED



PLAN
(SCALE: 1" = 40')
DENOTES: END OF BRIDGE DRAIN REQUIRED. SEE STD. DWG. NO. STD-1-6, 7, & 9.
DENOTES: BRIDGE DECK DRAIN REQUIRED. SEE STD. DWG. NO. STD-1-2.

LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT	M-331-83	1-15-98
GENERAL NOTES	M-331-84	1-15-98
ESTIMATED QUANTITIES	M-331-85	1-15-98
FOUNDATION DATA	M-331-86	
SUPERSTRUCTURE	M-331-87	1-15-98
SUPERSTRUCTURE DETAILS	M-331-88	
SUPERSTRUCTURE DETAILS	M-331-89	
SUPERSTRUCTURE DETAILS	M-331-90	7-16-96
SUPERSTRUCTURE DETAILS	M-331-91	1-15-98
BEARING DEVICE DETAILS	M-331-92	1-15-98
ERECTION DETAILS AT ABUT. NO. 1	M-331-93	1-15-98
ERECTION DETAILS AT PIER NO'S. 1-3	M-331-94	1-15-98
ABUTMENT NO. 1	M-331-95	1-15-98
ABUTMENT NO. 1 DETAILS	M-331-96	1-15-98
ABUTMENT NO. 1 DETAILS	M-331-97	1-15-98
ABUTMENT NO. 2	M-331-98	1-15-98
ABUTMENT NO. 2 DETAILS	M-331-99	1-15-98
ABUTMENT NO. 2 DETAILS	M-331-100	1-15-98
ABUTMENT NO. 2 DETAILS	M-331-101	1-15-98
PIER NO'S. 1-3	M-331-102	1-15-98
PIER NO'S. 1-3 DETAILS	M-331-103	1-15-98
FINAL FOUNDATION DATA	M-331-104	
BILL OF STEEL	M-331-105	1-15-98
BILL OF STEEL	M-331-106	1-15-98

LIST OF STANDARD DRAWINGS

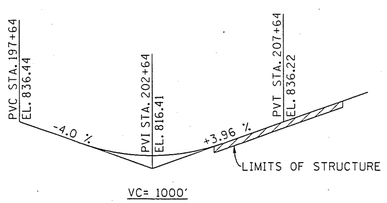
	DWG. NO.	LAST REV. DATE
* BRIDGE RAILING CONCRETE PARAPET	STD-1-1	12-18-95
* SLIDER PLATES AND DECK DRAINS	STD-1-2	9-11-95
* REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-5	12-18-95
BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-6	5-1-95
BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-7	5-1-95
BRIDGE END DRAIN DETAILS 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-9	5-1-95
STRIP SEAL EXPANSION JOINT NEW CONSTRUCTION	STD-3-1	2-14-94
STRIP SEAL EXPANSION JOINT NEW CONSTRUCTION	STD-3-2	2-14-94
STANDARD PILE DETAILS	STD-5-1	10-25-93
STANDARD PILE DETAILS	STD-5-2	10-26-92
STANDARD SEISMIC DETAILS	STD-6-1	5-01-95
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STD-10-1	5-11-92
SLOPE PROTECTION	RD-SA-1	4-29-88

LIST OF SPECIAL PROVISIONS

	PROV. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	3-06-95
STEEL STRUCTURES	602	3-18-96
COFFERDAMS AND FOUNDATION SEALS	604F	8-2-96
STRIP SEAL EXPANSION JOINTS	604S	2-19-96

* DENOTES: THESE DRAWINGS TO BE PRINTED WITH PLANS.

EXISTING BRIDGE DRAWINGS: K-26-96 THRU 102



GRADE SKETCH
(SOUTHBOUND LANE)
(ELEVATIONS BASED ON FINISHED GRADE)

HYDRAULIC DATA

DRAINAGE AREA = 518 sq. mi.
DESIGN DISCHARGE (100 YR.) = 39,900 cfs.
WATER AREA PROVIDED BELOW EL. 813.26 = 10,760 sq. ft.
100 YEAR VELOCITY = 3.71 ft./sec.
100 YEAR BRIDGE BACKWATER = 0.0 ft. AT EL. 813.26
ROADWAY OVERTOPPING EL. = 825.97
500 YEAR DISCHARGE = 51,260 cfs AT EL. 818.57

2016 ADT = 4,810
42'-0" ROADWAY W/STD-1-1 PARAPET
DESIGN SPEED = 60 MPH

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

SOUTHBOUND LANE
LAYOUT
STATE ROUTE 111
OVER CANEY FORK RIVER
BRIDGE ID. #88SR1110008
STATION 207+54.00 L.M. 19.5
VAN BUREN - WHITE COUNTIES
1996

CORRECT Edward A. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY	M. FLOWERS (PDD)	DATE	11-95
DRAWN BY	K.L.F. & C.A. BERNATEK	DATE	5-96
SUPERVISED BY	J. FIELDS/G. HALL	DATE	5-96
CHECKED BY	M. FLOWERS	DATE	6-96

M-331-83

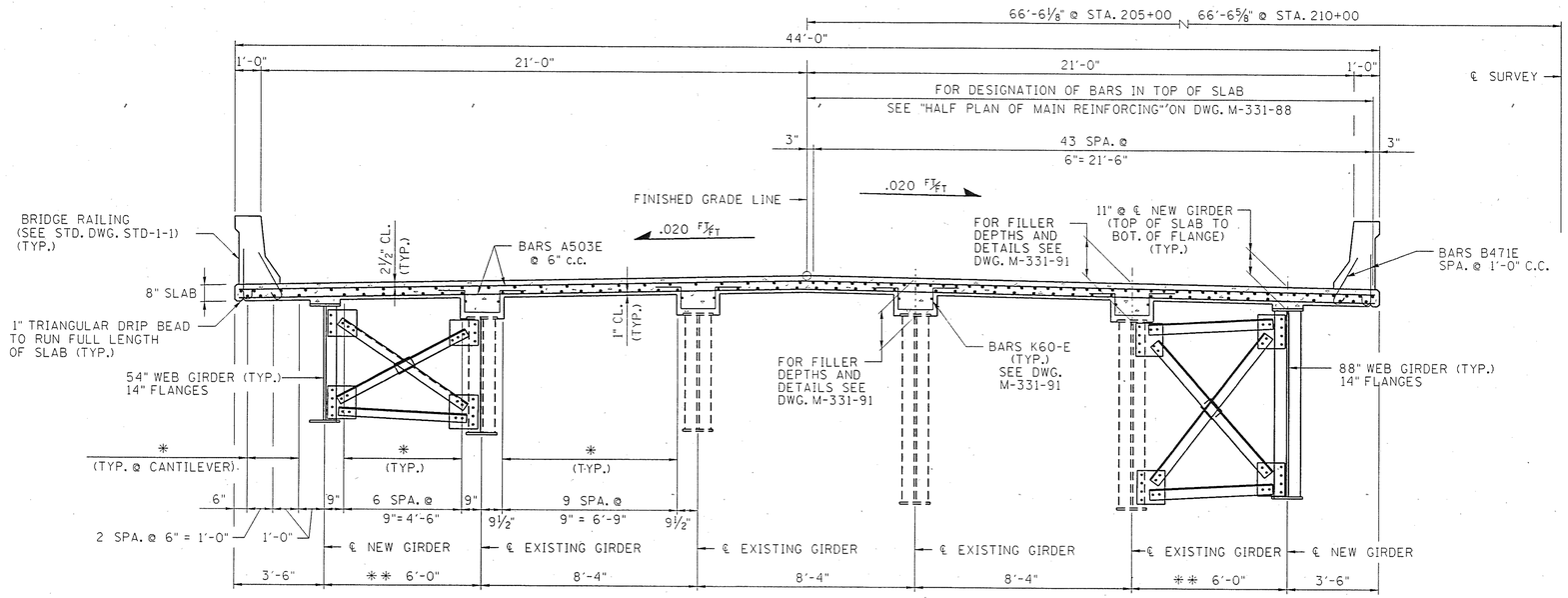
SP104F.DGN

CONST. NO. 88027-3227-64 & 93003-3217-64

PROJECT NO.	YEAR	SHEET NO.
APD-NH-111(29)	1996	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	1-15-98	MEF	REVISED STEEL QUANTITIES



HALF-SECTION NEAR MIDSPAN

HALF-SECTION NEAR BENT

TYPICAL CROSS SECTION

(LOOKING AHEAD ON SURVEY)

* BOTTOM LONGITUDINAL REINFORCEMENT:
BARS A501E ⅝" 17 BARS A500E ⅝" BARS A502E .
STAGGER SPLICES AS SHOWN .

** DENOTES: FOR CROSS-FRAME LOCATIONS AND DETAILS SEE DRAWINGS M-331-89 AND M-331-90 .

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: THE CONCRETE DECK SHALL NOT BE POURED UNTIL ALL STRUCTURAL STEEL IS ERECTED AND ALL WELDING AND/OR BOLTING COMPLETE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE DRAWING NO. M-331-87, 97, AND 101 .

APPROVAL OF MATERIALS: NO FABRICATION SHALL BE STARTED UNTIL THE MATERIALS INVOLVED HAVE BEEN APPROVED BY THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS, WITH A COPY OF THE TEST REPORTS ALSO GOING TO THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS.

IDENTITY OF MAIN MATERIALS: SEE SPECIAL PROVISION NO. 602.

STRUCTURAL STEEL:
SHALL CONFORM TO AASHTO M 270 (ASTM A709) GRADE 36 UNLESS OTHERWISE NOTED. ALL STRUCTURAL STEEL FOR GIRDER FLANGES IN TENSION AND ALL WEBS SHALL MEET THE SUPPLEMENTAL REQUIREMENTS FOR LONGITUDINAL CHARPY V-NOTCH TESTS SPECIFIED IN AASHTO MATERIAL SPECIFICATIONS. ZONE 2 OF NON-FRACTURE CRITICAL CRITERIA SHALL APPLY.

STRUCTURAL STEEL DESIGNATED ON THE PLANS AS FRACTURE CRITICAL SHALL MEET THE REQUIREMENTS OF AASHTO GUIDE SPECIFICATIONS FOR FRACTURE CRITICAL NON-REDUNDANT STEEL BRIDGE MEMBERS AS REQUIRED FOR ZONE 2. FABRICATION OF FRACTURE CRITICAL BRIDGE MEMBERS SHALL BE ACCOMPLISHED BY FABRICATORS CERTIFIED UNDER THE AISC QUALITY CERTIFICATION PROGRAM, CATEGORY III, MAJOR STEEL BRIDES. NO OTHER CERTIFICATION PROGRAM WILL BE ACCEPTABLE.

WELDING: ANSI/AASHTO/AWS D1.5-88 BRIDGE WELDING CODE AND SPECIAL PROVISION 602.

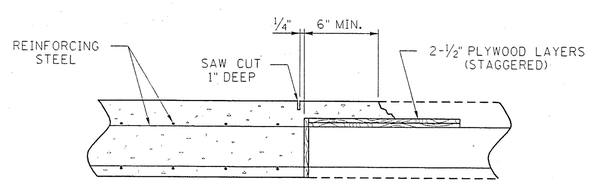
FIELD CONNECTIONS: SHALL BE ⅞" ø HIGH TENSILE STRENGTH BOLTS ASTM A325 UNLESS OTHERWISE SHOWN. SEE AASHTO SPECIFICATIONS ART. 11.3.2 DIVISION II AND SPECIAL PROVISION 602.

ADDITIONAL SHOP SPLICE NOTE: SHOP SPLICES NECESSARY DUE TO LENGTHS OR SIZE OF MATERIAL INVOLVED MAY BE ADDED BY THE FABRICATOR SUBJECT TO APPROVAL BY THE ENGINEER AND SHALL BE AT NO ADDITIONAL COST TO THE PROJECT.

ADDITIONAL FIELD SPLICE NOTE: FIELD SPLICES NECESSARY DUE TO LENGTHS INVOLVED MAY BE ADDED BY THE FABRICATOR SUBJECT TO APPROVAL BY THE ENGINEER AND SHALL BE AT NO ADDITIONAL COST TO THE PROJECT.

SHOP ASSEMBLY: PROGRESSIVE SHOP ASSEMBLY WILL BE ALLOWED. SEE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, ART. 11.5.3.1 DIVISION II.

PLANS NOTE: THE SLAB SHALL NOT BE POURED UNTIL ALL STRUCURAL STEEL IS ERECTED AND ALL WELDING OR BOLTING COMPLETE. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT THE EXTERIOR BEAM FROM TWISTING. NO PORTION OF THE CURB OR PARAPET SHALL BE POURED UNTIL THE ENTIRE SLAB IS IN PLACE.



SLAB CONSTRUCTION JOINT DETAIL

NOTE: ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THIS DETAIL . FOR DECK CONCRETE POURING SEQUENCE SEE DWG. M-331-91 .

ESTIMATED QUANTITIES	
CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	⚠ EPOXY COATED REINFORCING STEEL LB.
618	163,516

ROBERT M. DISHNER REGISTERED ENGINEER No. 5758 STATE OF TENNESSEE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

SOUTHBOUND LANE
SUPERSTRUCTURE
STATE ROUTE 111
OVER CANEY FORK RIVER
STATION 207+54.00
VAN BUREN - WHITE COUNTIES
1996

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY	M. FLOWERS	DATE	4-'96
DRAWN BY	T. LEWIS	DATE	4-'96
SUPERVISED BY	J.FIELDS & G.HALL	DATE	4-'96
CHECKED BY	M. FLOWERS	DATE	6-'96

M-331-87